

CITY PLANNING COMMISSION

1231 'I' STREET, SUITE 200, SACRAMENTO, CA 95814

APPLICANT Timothy J. Powell - 2120 N. Street #1, Sacramento, CA 95816
OWNER Timothy J. Powell - 2120 N. Street #1, Sacramento, CA 95816
PLANS BY Timothy J. Powell - 2120 N. Street #1, Sacramento, CA 95816
FILING DATE 11-14-88 ENVIR. DET. Ex. 15303a REPORT BY W:sg
ASSESSOR'S PCL. NO. 275-0104-005

APPLICATION:

- A. Special Permit to convert an existing 720 sq. ft. garage into a second residential unit on 0.15+ developed acres in the Standard Single Family (R-1) zone
- B. Variance to waive the required covered, enclosed garage for a second residential unit
- C. Variance to establish a second residential unit which exceeds the maximum 640 sq. ft. by 80 sq. ft.
- D. Variance to reduce the required side yard setback by 1-1/2 ft. from 5 ft. to 3-1/2 ft.
- E. Variance to reduce the required rear yard setback by 3 ft. from 15 ft. to 12 ft.

LOCATION: 2361 Beaumont Street

PROPOSAL: The applicant is requesting the necessary entitlements to allow an existing second residential unit.

PROJECT INFORMATION:

General Plan Designation: Low Density Residential (4-15 du/na)
1984 North Sacramento Community
Plan Designation: Residential (4-8 du/na)
Existing Zoning of Site: R-1
Existing Land Use of Site: Two residential units

Surrounding Land Use and Zoning:	Setbacks:	Required	Provided
North: Residential; R-1	<u>Second Unit</u>		
South: Residential; R-1	Side(Int):	5'	3.5'
East: Residential; R-1	Rear:	15'	12'
West: Residential; R-1			

Parking Required: 2
Parking Provided: 1 (driveway)
Property Dimensions: 50' x 12'
Property Area: 0.15+ acres
Square Footage of Building: Second unit - 720 sq. ft.
Height of Building: Second unit - 12'

APPLC. NO. P88-477 MEETING DATE 2-9-89 ~~1/26/89~~ January 17, 1989 ITEM NO. 18 ⁹

Topography:	Flat
Street Improvements:	Existing
Utilities:	Existing
Exterior Building Materials:	Wood siding
Roof Material:	Composition shingle

PROJECT EVALUATION: Staff has the following comments:

A. Land Use and Zoning

The subject site consists of a 50' x 127' site of 0.15+ acres in the Standard Single Family (R-1) zone. The site is an interior lot developed with a single family residence and a second residential unit to the rear consisting of 720 square feet. The site is designated residential by both the General Plan and the 1984 North Sacramento Community Plan. Surrounding land uses and zoning include residential to the north, south, east and west zoned R-1.

B. Applicant's Proposal

The applicant is proposing to allow a 720 square foot residential unit on a 0.15+ acre developed lot. The applicant is requesting a variance to waive the required covered, enclosed garage for a second residential unit. A variance to allow a second residential unit which exceeds the maximum 640 square feet by 80 square feet is requested by the applicant. In addition, the applicant is requesting a variance to reduce the required 5' side yard setback to 3-1/2' and to reduce the required 15' rear yard setback to 12'. The applicant has indicated to staff that these requests are necessary to allow the new second residential unit to become legal.

C. Garage Conversion

In March 1985, the applicant requested a building permit to build a 20' x 36' workshop and storage unit behind the existing residential structure on the subject site. The building permit allowed the applicant to convert the garage into a workshop/storage use only. On February 26, 1986 the workshop/storage structure received final building inspection and was approved by the Building Division. The applicant illegally modified the structure to become a second residential unit. The unit has been rented for 1-1/2 years. Recently, the City inspectors were notified of the illegal structure which is in violation of the building codes.

Section 2E(26)(i) of the Zoning Ordinance requires that a dwelling or mobile home must have an enclosed garage (either attached or detached) if more than 50% of other dwellings or mobile homes located within 1,000', measured structure to structure, have enclosed garages. Staff surveyed the area and found that the majority of the homes in the area have one car garages, carports or garages which have been converted into storage spaces. The abutting property to the north of the subject site has an enclosed garage which appears to be in very good condition. The applicant is, therefore, required to convert the second residential structure back into a workshop/storage space or a one car garage.

Staff does not support a variance to waive the required covered, enclosed garage into a second residential unit or a special permit to convert the structure into a 720 square foot residential unit.

A second residential unit is allowed on an R-1 zoned lot subject to a special permit. The second unit shall not exceed 640 square feet; the unit shall not cover more than 25% of the rear yard area; and two parking spaces shall be provided on the lot. Duplexes on corner lots in an R-1 zone are permitted by right and are not required by Section 2E(26)(i) to have a garage if more than 50% of the dwellings within 1,000 feet have garages. Staff suggested that the applicant modify the illegal structure into a one car garage and studio use provided that an additional paved parking space is placed on the site. Staff noticed a vehicle in the rear yard area on grass and gravel. The applicant was not willing to modify the structure. Staff has recommended approval of a number of second residential units on interior lots in an R-1 zone. The project, however, retained two car garages, built new garages, or made modifications to the structure to comply with City standards.

D. Side Yard and Rear Yard Setbacks

A second residential unit in an R-1 zone has a required 5 ft. side yard setback and a 15 ft. rear yard setback. The applicant is proposing to provide a 3-1/2 ft. side yard setback and a 12 ft. rear yard setback which are the existing setbacks of the secondary unit. In previous second residential unit approvals, projects on interior lots were able to provide the required rear yard and side yard setbacks. Some projects on corner lots have been granted a variance to reduce the rear yard or side yard setback. Due to the size of the illegal structure, approximately 96% of the rear yard area of the subject site is covered. Only 25% is allowed. Staff does not support the variance to reduce the rear yard and side yard setbacks for the interior lot in an R-1 zone. The proposed setbacks would result in an adverse impact for the existing residential unit on the subject site.

E. Secondary Residential unit Criteria/Size of Lot

A special permit is required to develop a second unit on an interior lot. A special permit shall not be granted for a second residential unit unless the following four criteria are met:

1. The architecture shall be compatible with the main residential unit.
2. Parking requirements of the Zoning Ordinance shall be met.
3. The height, lot coverage and setback requirements shall be met.
4. The area of the second residential unit shall not exceed 640 square feet.

Staff finds that criteria 2, 3 and 4 above will not be met and thus cannot support the request for a special permit. All other requests for a special

permit to construct a second residential unit of previous projects were able to meet all four criteria.

Planning staff finds that the subject site is too small to accommodate a second residential unit as proposed and comply with zoning regulations. Similar past projects located on an interior lot in the R-1 zone had lot depths of 135 ft. to 158 ft. These projects have been able to comply with setbacks, some by going to a two-story structure or adding directly onto an existing structure. Approval of the subject project would set an undesirable precedent of allowing demolition of existing garages and construction of secondary units that do not meet setbacks or parking requirements and exceeds the required 640 sq. ft. In addition, the existing second residential structure was illegally constructed.

F. Agency Comments

The proposal was routed to the City's Traffic Engineer, Engineering, Building Department, City Water and Sewer, and City Real Estate. No comments were received.

ENVIRONMENTAL DETERMINATION: This project is exempt from environmental review pursuant to State EIR Guidelines (CEQA Section 15303(a)).

RECOMMENDATION: Staff recommends that the Commission take the following action:

- A. Deny the special permit to convert an existing 720 sq. ft. garage into a second residential unit based on findings of fact which follow;
- B. Deny the variance to waive the required covered, enclosed garage for a second residential unit based on findings of fact;
- C. Deny the variance to establish a second residential unit which exceeds the required maximum 640 sq. ft. based on findings of fact;
- D. Deny the variance to allow a second residential unit within the required 5 ft. side yard setback based on findings of fact which follow; and
- E. Deny the variance to allow a second residential unit within the required 15 ft. rear yard setback based on findings of fact which follow.

Findings of Fact - Special Permit

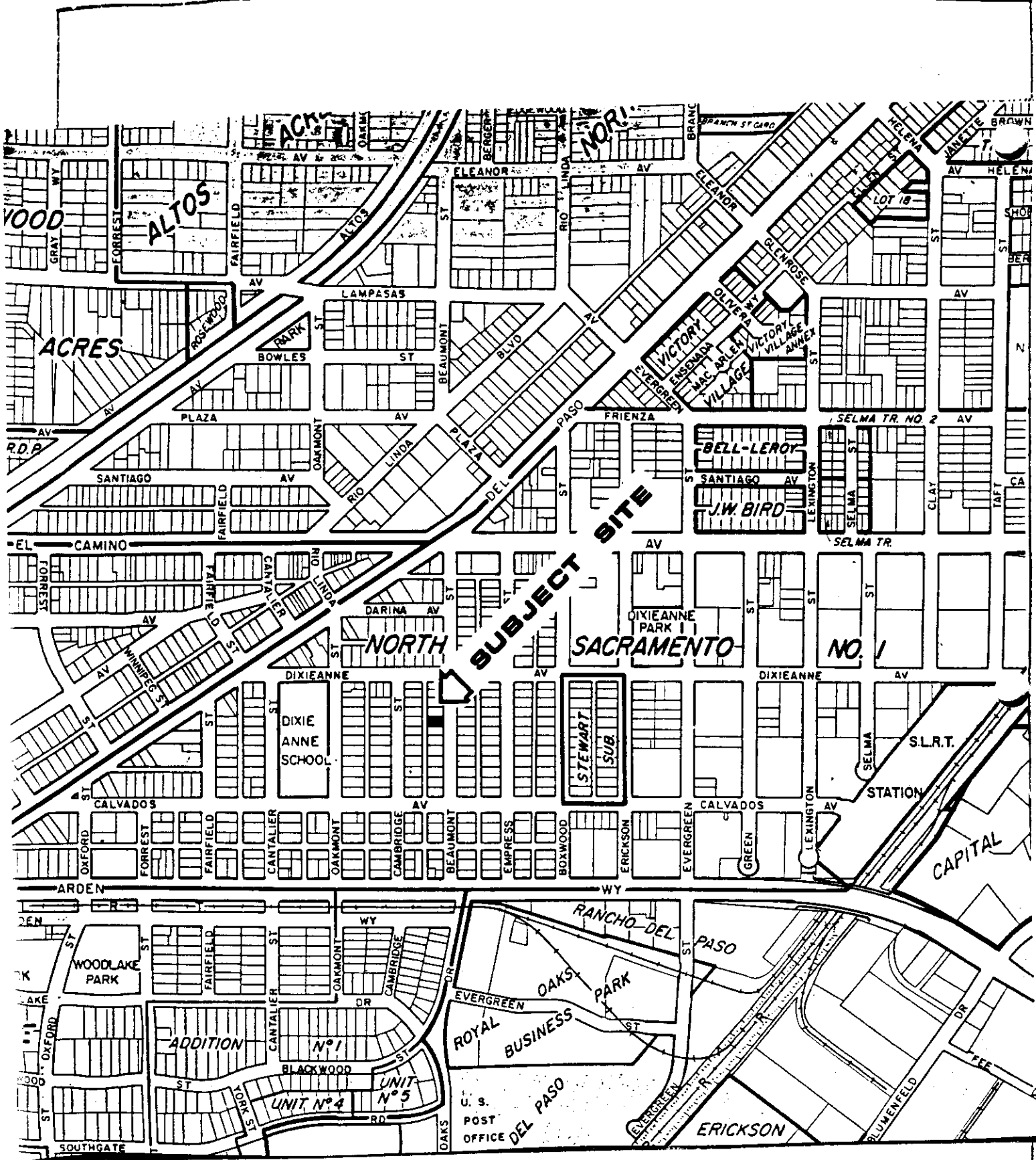
1. The project is not based on sound principles of land use, in that:
 - a. there is insufficient area on the subject site to accommodate a detached second residential unit while meeting zoning requirements; and
 - b. the proposed use is not compatible with existing storage/workshops and one car garage uses in the area.

2. The project would be detrimental to the public health, safety and welfare, in that:

a precedent would be set to allow conversion of necessary parking garages in order to illegally construct second residential units, which could lead to significant density increases and parking shortages in a single family area.

Findings of Fact - Variances

1. Granting the variances would constitute a special privilege extended to an individual applicant in that:
 - a. a variance would not be granted to other property owners facing similar circumstances;
 - b. the structure was illegally converted after final building inspection for a workshop and storage area;
 - c. the applicant is not willing to modify nor add another parking space on the subject site; and
 - d. there is no hardship involved to support the request.
2. Granting the variances would be injurious to the public health, safety, and welfare, in that inadequate setbacks and parking would promote future surrounding residences to develop a similar structure.



VICINITY MAP



P88-477

~~1012-89~~ 2-9-89

Item 189

PUBLIC ALLEY 20'

PARKING

12'

PROPOSED UNIT

20' x 36'

CON/SLAB

3'6"

NORTH →

SITE PLAN

SCALE 1" = 16'

LOT 50' x 127 1/2'

18'

EXISTING HOUSE

EXISTING DRIVEWAY

CPC FILE COPY

APPL NO. P 88-77

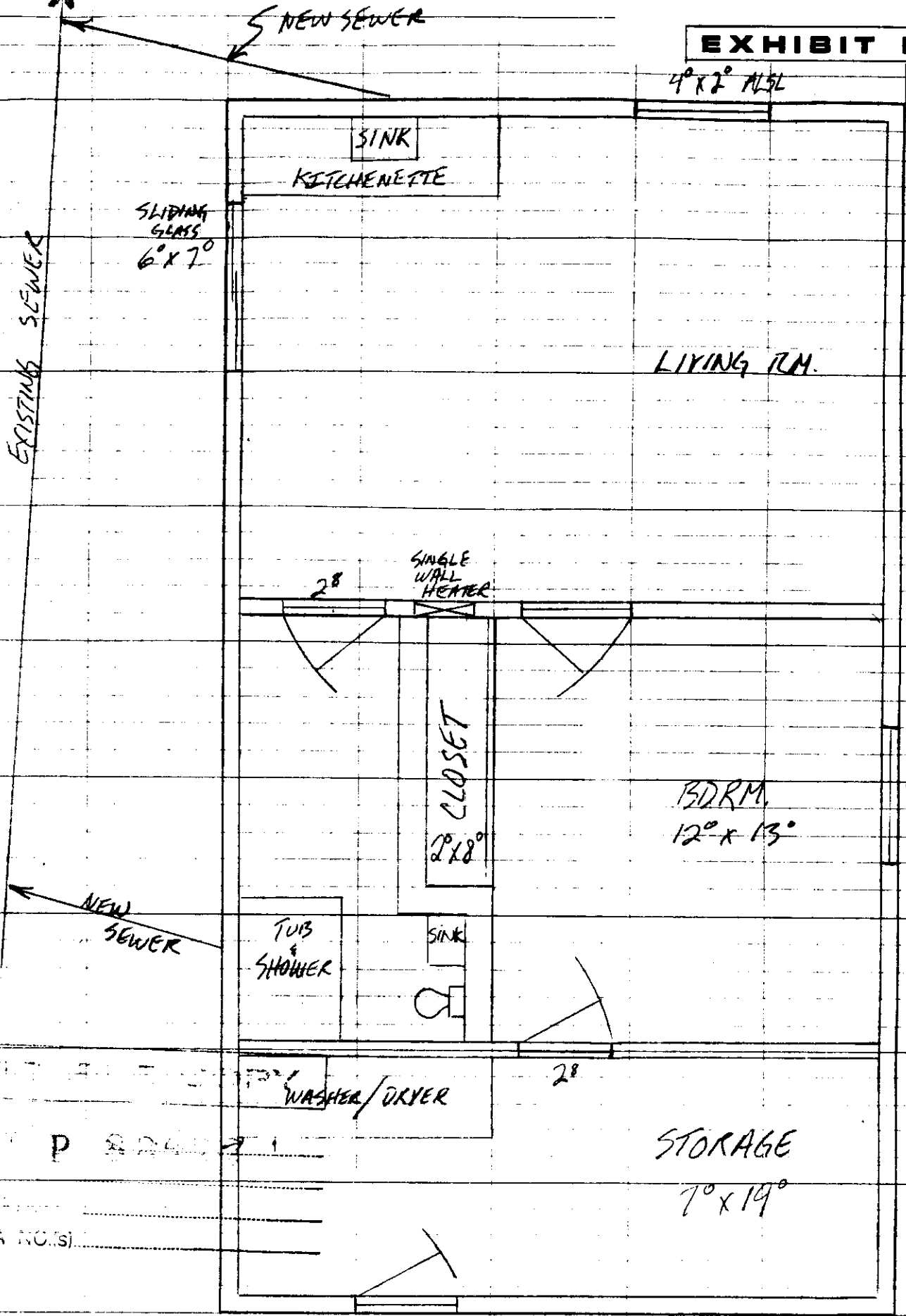
EXHIBIT 3

MTC DATE

AGENDA NO. (S)

BEAUMONT STREET
PARCEL # 275-0104-005

EXHIBIT B

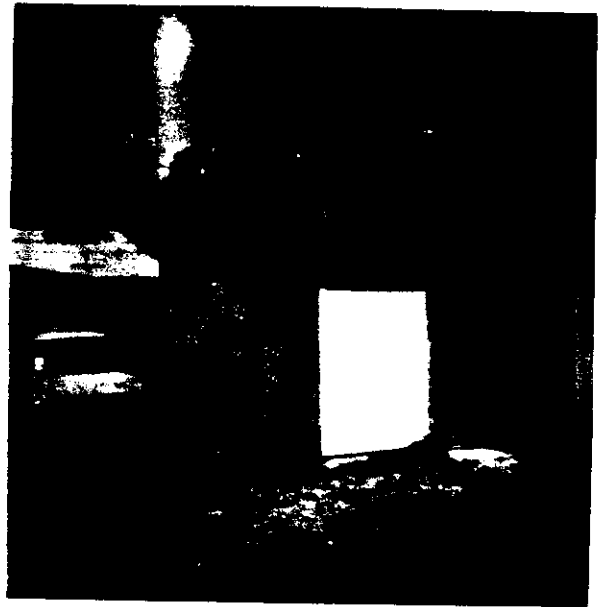
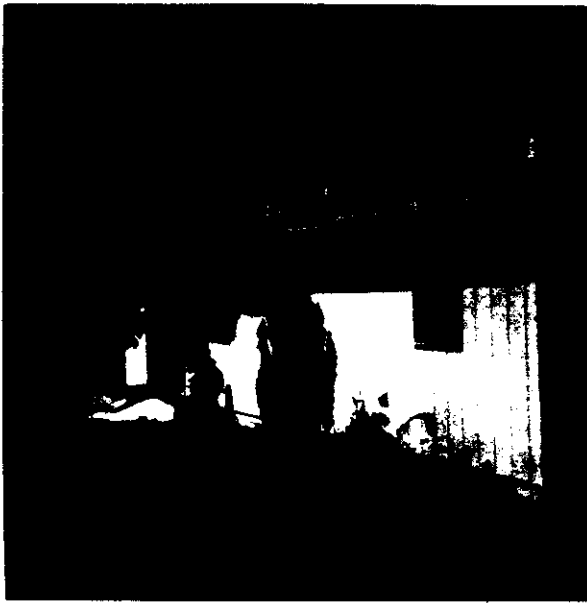
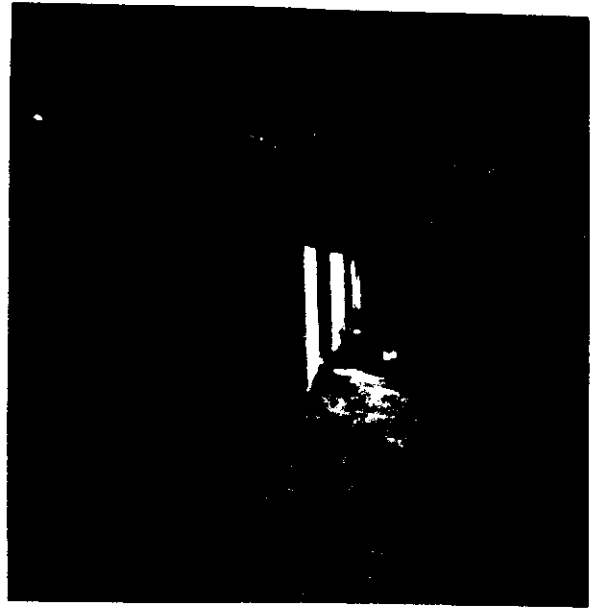


FLOOR PLAN - PROPOSED UNIT
 20' x 36'

SCALE 1" = 4'0"
 North →
 Item #89

AGENDA NO. (S)

EXHIBIT C



177

264
~~HA 89~~
2-8-88

Item 18