

# CITY PLANNING COMMISSION

1231 "I" STREET, SUITE 200, SACRAMENTO, CA 95814

**APPLICANT** ENTEC, 1555 River Park Drive, Suite #206, Sacramento, CA 95815  
**OWNER** Maverick Properties, Inc. 2914 Pasatiempo Place, Sacramento, CA 95833  
**PLANS BY** ENTEC, 1555 River Park Drive, Suite #206, Sacramento, CA 95815  
**FILING DATE** 8/7/87 **ENVIR. DET.** EX 15303b **REPORT BY** JP/vf  
**ASSESSOR'S PCL. NO.** 274-270-006

- APPLICATION:**
- A. Tentative Map to divide 0.7 $\pm$  vacant acres into three lots in the Flood (F) zone.
  - B. Special Permit to develop a six-unit, 6,048 $\pm$  sq. ft. airspace condominium development.
  - C. Subdivision Modification to waive standard street improvements.

**LOCATION:** South side of Garden Highway at the terminus of Orchard Lane.

**PROPOSAL:** The applicant is requesting the necessary entitlements to construct a six unit condominium development on the 0.7 $\pm$  acre site.

**PROJECT INFORMATION:**

Interim Land Use Policy: Riverfront District  
Existing Zoning of Site: Flood  
Existing Land Use of Site: Vacant

**Surrounding Land Use and zoning:**

North: Swallows Nest Condominiums; R-1A  
South: Sacramento River; F  
East : Vacant, Single Family; F  
West : Vacant, Riverview Marina Condominiums; F

Parking Required: 10 spaces  
Parking Provided: 12 spaces  
Property Dimensions: Irregular  
Property Area: 0.7 $\pm$  acre  
Density of Development: 8.5 d. u. per acre  
Square Footage of Building: 6,048 $\pm$  per acre  
Height of Building: 3 stories  
Exterior Building Materials: Wood shiplap siding  
Roof Material: Metal

**SUBDIVISION REVIEW COMMITTEE RECOMMENDATION:** On September 9, 1987, by a vote of five ayes and four absent the Subdivision Review Committee voted to recommend approval of the tentative map subject to the attached conditions.

**PROJECT EVALUATION:** Staff has the following comments regarding this proposal:

**APPLC. NO.** P87-359 **MEETING DATE** 9/24/87 **ITEM NO** 16

A. Existing Land Use and Zoning

The subject site consists of 0.7± vacant acres located in the Flood (F) zone. Surrounding land uses are the Swallows Nest Condominiums zoned R-1A to the north, the Sacramento River to the south, a single family residence zoned F to the east and the 13 unit Riverview Condominium and Marina project zoned F to the west. The site is designated as the Riverfront District by the City's Interim Land Use Policy.

B. Applicant's Proposal

The applicant proposes to divide the site into three lots and construct a six unit, 6,048± sq. ft. airspace condominium complex on the site. The airspace condominium units would be located on Lot A & Lot B is proposed as the common area for the project. Development is not proposed for Lot C at this time. An existing oak grove would be preserved on this lot.

Residential developments are allowed in the Riverfront District and in the Flood zone with special permit approval. Planning staff finds the project to be an appropriate land use for the site if the tentative map is revised to show a one lot subdivision. Presently Lot C provides the only usable open space area for the project and provides the public view of the Sacramento River from the Garden Highway as required by the Riverfront District designation. The proposed density of the project is 8.5 dwelling units per acre when the acreage of the entire site (0.7± acres) is used. This density is compatible with the densities of the two adjacent condominium developments, Riverbank Holding Company and Riverview Marina. If the Lot C acreage is removed from the calculations and allowed to develop separately the density of the condominium development would increase to 12 dwelling units per acre. Planning staff finds that six units on 0.5± acres with little usable open space would be too intense of a project for the site. The applicant has indicated that he is willing to revise the map to indicate a one lot subdivision as recommended by Planning staff.

C. Site Plan Design

The applicant's site plan indicates the retention of the existing oak trees on the eastern portion of the site (Exhibit B). No other landscaping has been indicated. Planning staff recommends that a revised landscape and irrigation plan be submitted for Planning Director review and approval. The landscape plan should include: 1) the retention of all natural vegetation and inclusion of new vegetation as required by interested State and Federal Agencies; 2) a minimum four foot wide planter between the public right-of-way and any paved areas; and 3) a landscape plan for the western portion of the site which provides a usable open space area for the condominium residents.

Driveway entrances for the project as shown on the submitted site plan are inadequate. Two-way driveways must be a minimum of 24 feet wide. If driveways are proposed to be one-way only, the circulation plan must receive the approval of the City Traffic Engineer. The two proposed driveways for the project will need to be revised to the satisfaction of the Traffic Engineering Division prior to issuance of building permits.

A solid wall is indicated on the site plan adjacent to the public right-of-way. Any fencing in this area must meet the approval of the State Reclamation Board. Any fencing must also meet the visibility requirements of the Zoning Ordinance and Traffic Engineering Division. The Fire Department has indicated that they will not permit any obstacles in the driveway area adjacent to the condominium structures. The proposed fence in this area will need to be removed.

A subdivision modification to waive standard street improvements has been requested for the proposed development. Due to the unique character of the projects adjacent to the Garden Highway, staff has no objection to the waiver of curb, gutter and sidewalk. Any necessary improvements shall be to the satisfaction of the Director of Public Works.

D. Building Design

The applicant has met with Planning staff several times to discuss the proposed building elevations. The proposed units are three (Exhibits C-E) stories, 29 feet high measured from the Garden Highway side of the structure to the top of the plate line. Two of the units have three bedrooms, the remaining four units have two bedrooms. Each level of each unit has an outdoor balcony facing the river. Each unit also has a two car garage. Proposed building materials are wood shiplap siding and a metal roof. In order to avoid one long building facade, the applicant has off-set units 5 and 6 from the rest of the four units and provided different roof pitches.

Planning staff finds that the applicant has made substantial progress in designing a residential project that is compatible with the theme of the Riverfront District. Some more detailing, however, is needed on the elevations. Staff recommends that revised plans and elevations be submitted for the review of the City Design Review staff prior to issuance of building permits.

ENVIRONMENTAL DETERMINATION: This project is exempt from environmental review pursuant to State EIR Guidelines (CEQA Section 15303(b)).

RECOMMENDATION: Staff recommends that the Planning Commission take the follow actions:

- A. Recommend approval of the tentative map, subject to conditions.

- B. Approve the special permit to develop a six unit, 6,048± square foot airspace condominium development in the F zone, subject to conditions and based upon findings of fact which follow.
- C. Recommend approval of the subdivision modification to waive standard street improvements.

Conditions - Tentative Map

The applicant shall satisfy each of the following conditions prior to filing the final map unless a different time for compliance is specifically noted:

1. The map shall be revised to show a one lot subdivision;
2. Provide street lights pursuant to Section 40.811 of the City Code;
3. Prepare a sewer and drainage study for the review and approval of the City Engineer; may require off-site extensions and oversizing;
4. Pay off existing assessments, or file the necessary segregation requests and fees to segregate existing assessments if necessary;
5. Pursuant to City Code Section 40.1302 (Parkland Dedication), the applicant shall submit to the City an appraisal of the property to be subdivided and pay the required parkland dedication in-lieu fees. The appraisal shall be dated not more than 90 days prior to the filing of the final map;
6. Meet all County Sanitation District requirements;
7. Widen pavement and improve Garden Highway as per study on file with the City. Right-of-way study required. Dedicate right-of-way as indicated;
8. Obtain approval of RD 1000 and pay necessary fees;
9. Record a parking maintenance agreement addressing maintenance, ingress and egress between homeowners;
10. Show reciprocal access, sewer, water and drainage easements on final map;
11. Dedicate a 10 ft. maintenance and levee easement at the toe of the levee;

12. All water and sewer crossings of the levee shall be privately constructed and maintained. Coordinate with the Army Corp of Engineers and State Reclamation Board;
13. Remove garbage and trash to an approved dump site to the satisfaction of the Planning Director and City/County Health Department;
14. Erect a chain link fence around the dripline of the Oak Grove on Lot C during construction; and
15. Place a note on the final map indicating that no trees in Oak Grove shall be removed without the written permission of the City Arborist and Planning Director.

\* **INFORMATIONAL ITEM:** Zone AE - BFE 31 should be checked by Public Works.

Conditions - Special Permit

1. The applicant shall obtain the necessary permits for development adjacent to the Sacramento River as mandated by the Federal and State Permitting Agencies (US Army Corps of Engineers, State Reclamation Board, Reclamation District 1000, State Department of Fish and Game, State Lands Commission) and shall be accomplished prior to issuance of a building permit.
2. Development of the condominiums shall be supported by a post and beam construction with finished floor levels above the 100 year floodplain.
3. Revised landscape and irrigation plans shall be submitted for the review and approval of the Planning Director prior to issuance of building permits. The landscape plans shall indicate the following:
  - a. retention of natural vegetation where possible and as required by interested State and Federal agencies. Where removal of natural vegetation is necessary it shall be subject to the review and approval of the City Arborist and Planning Director;
  - b. retention of the existing Oak Grove;
  - c. a minimum four foot wide planter between the public right-of-way and any paved areas; and
  - d. a landscape plan for the western portion of the site which provides a usable open space area for condominium residents.

4. Construction work and placement of impervious surfaces shall be approved by the City Arborist.
5. The applicant shall submit the detailed plans which indicate the driveway locations for the review and approval of the City Traffic Engineer prior to issuance of building permits.
6. The design and materials of the fence proposed adjacent to the Garden Highway right-of-way shall be subject to the review and approval of the Planning Director prior to issuance of building permits. The proposed fence shall also meet the requirements of the State Reclamation Board, City Fire Department, and the visibility requirements of the Zoning Ordinance and Traffic Engineering Division.
7. Revised elevations for the project shall be submitted for the review and approval of City Design Review staff prior to issuance of building permits.
8. The water supply, on-site fire hydrants and fire fighting equipment shall comply with the requirements of the City Fire and Engineering Departments.
9. Exterior lighting shall be shielded to eliminate glare toward the river.
10. If unusual amounts of bone, stone, or artifacts are uncovered, work within 50 meters of the area will cease immediately and a qualified archaeologist shall be consulted to develop, if necessary, further mitigation measures to reduce any archaeological impacts to less than significant effect before construction resumes.
11. The applicant shall properly compact fills, periodically moisten the exposed soil areas and/or landscape exposed soils during construction to reduce water and wind-caused erosion.
12. Exposed riverbanks and the levee shall be protected from erosion control features, subject to the review and approval of the Planning Director and the State Board of Reclamation.
13. A flood evacuation plan shall be included as part of the C.C. and R's.
14. The applicant shall provide a drainage and sanitary sewer system designed for the runoff of the full development for the review and approval of the City Engineer prior to any construction on the site.

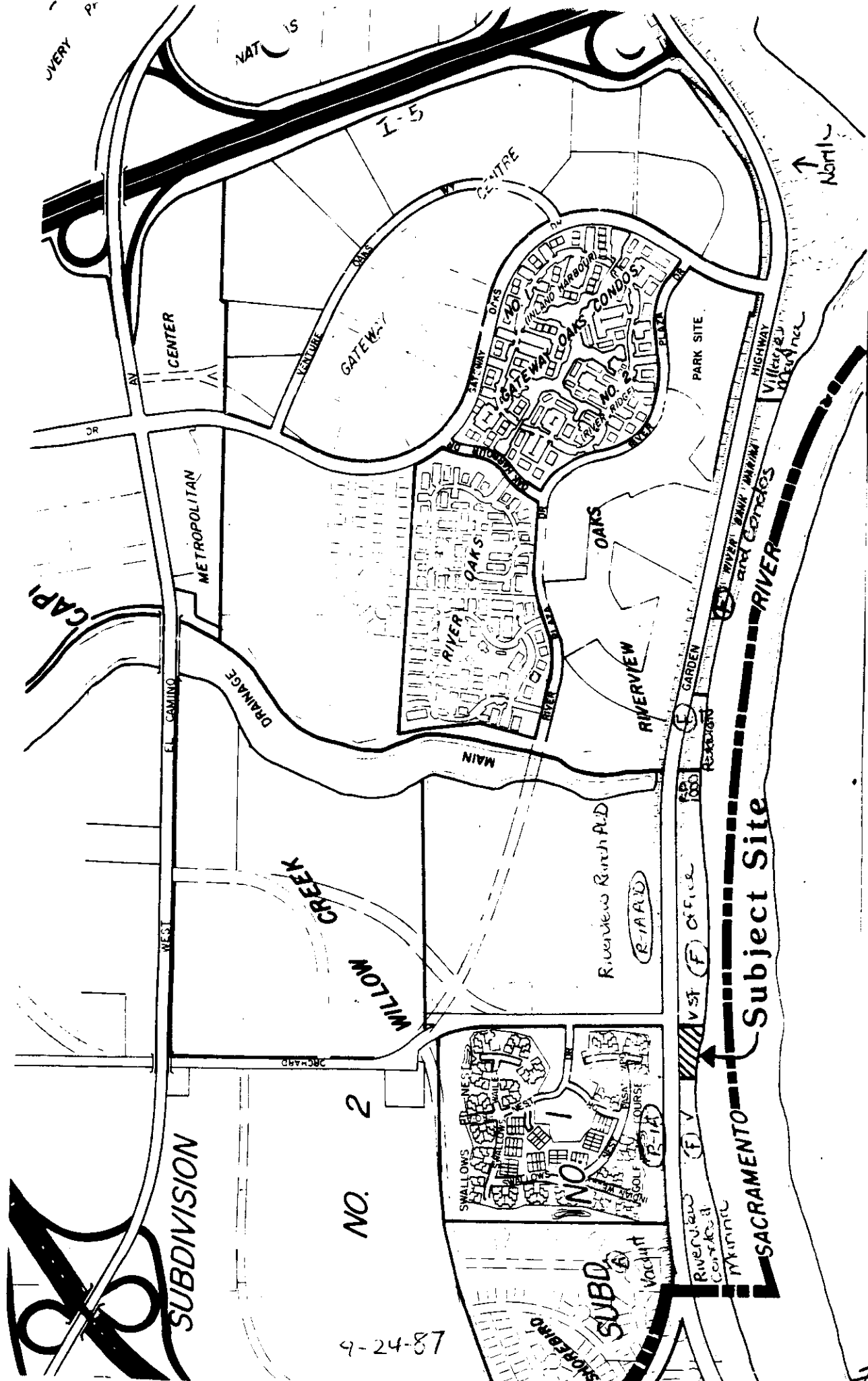
15. To reduce erosion and confine sedimentation, the applicant shall provide vegetation of exposed areas, debris collection areas, sediment basins and/or mulching on the site.
16. Excavation work shall be conducted during low water periods to prevent excessive sediment discharge.

Findings of Fact

1. The proposed project is based on sound principles of land use in that:
  - a) the zoning ordinance allows the uses in the flood zones with the approval of a special permit.
  - b) the proposal complies with the criteria for "Special Developments in the Flood Zones" in that:

the development will have direct access to a major collector street; adequate on-site parking is provided; the design of the structures will be compatible with the riverfront character and surrounding properties; and the project provides a public view of the Sacramento River from the Garden Highway as required by the riverfront District.
2. The project is not injurious to the general public or surrounding properties in that:
  - a) the structures will be constructed above the 100 year flood level;
  - b) the site will meet on-site fire protection requirements;
  - c) the proposal is subject to required State and Federal approvals for development along the Sacramento River;
  - d) adequate on-site parking and landscaping shall be provided.
3. The proposed project is consistent with the City's Discretionary Interim Land Use Policy in that the site is designated as the Riverfront District by the City's Interim Land Use Policy and the proposed residential use is allowed in this designation.

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VICINITY - LAND USE - ZONING

NO. 2

9-24-87

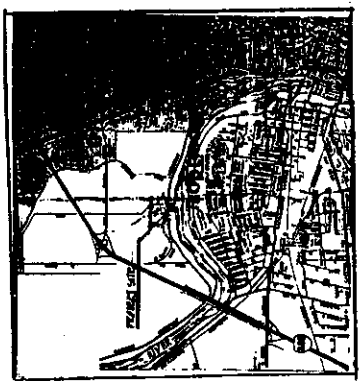
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LOCATION MAP

RIVERVIEW RANCH

ORCHARD LANE

MAIN CENTER LINE CONNECTION TO NORTH 50'S FROM NORTH

SWALLOW NEST VILLAGE

R.O.W.

GARDEN HIGHWAY

CRAW

CONTINENTAL LEASING COMPANY

SACRAMENTO-SAN JOAQUIN DRAINAGE DISTRICT

RIVER

SACRAMENTO

**GENERAL INFORMATION**

OWNER: AVERILL PROPERTIES, INC.  
 1000 J STREET, SUITE 200  
 SACRAMENTO, CA 95811  
 PHONE: (916) 422-2313

AGENT: SUE WOOD PAUL BULLIE  
 1000 J STREET, SUITE 200  
 SACRAMENTO, CA 95811  
 PHONE: (916) 422-2313

PARCEL NO.: 274-210-06  
 ADDRESS: 0.1 +/- ACRES  
 EXISTING USE: TRACT  
 EXISTING ZONE: R1A  
 PROPOSED USE: MULTI-FAMILY RESIDENTIAL  
 PROPOSED ZONE: R1A  
 NUMBER OF UNITS: FOUR  
 LOT SIZE: 0.1 ACRES  
 WATER SUPPLY: CITY OF SACRAMENTO  
 SOIL SURVEY: COUNTY OF SACRAMENTO  
 FIRE PROTECTION: SACRAMENTO FIRE DISTRICT

**TREE LEGEND**

NO.	SP. TYPE	HEIGHT (FEET)
1	10' PAK	10'
2	12' PAK	12'
3	14' PAK	14'
4	16' PAK	16'
5	18' PAK	18'
6	20' PAK	20'
7	22' PAK	22'
8	24' PAK	24'

**SYMBOL**

—	EAST OF WAY
—	PROPERTY LINE
—	EXISTING PROPERTY
—	NEW PROPERTY
—	PAVED DRIVEWAY
—	EXISTING TREE
—	BLANK
—	NEW

TENTATIVE SUBDIVISION MAP  
 OF  
**THE SUTTER KINGS**  
 A PORTION OF LOT 'F' OF SWALLOW'S NEST UNIT NO. 1  
 BOOK 131 OF MAPS, MAP NO. 2  
 COUNTY OF SACRAMENTO, CALIFORNIA  
 JUNE 1987

PLEASE: THE PURPOSE OF THIS MAP IS TO CREATE AN AIR SPACE CONDOMINIUM SUBDIVISION ON LOT 'F' WITH LOT 'B' REMAINING UNDIVIDED. THIS MAP IS TO BE FILED AND THE AIR SPACE CONDOMINIUM SUBDIVISION SHALL BE RECORDED IN THE PUBLIC RECORDS OF THE COUNTY OF SACRAMENTO.

THE SUTTER KINGS CONDOMINIUMS  
 PARCEL NO. 274-210-06, SWALLOW'S NEST UNIT NO. 1, LOT 'F'

TENTATIVE SUBDIVISION MAP



ASSOCIATE  
 R. S. Bullie  
 C27424

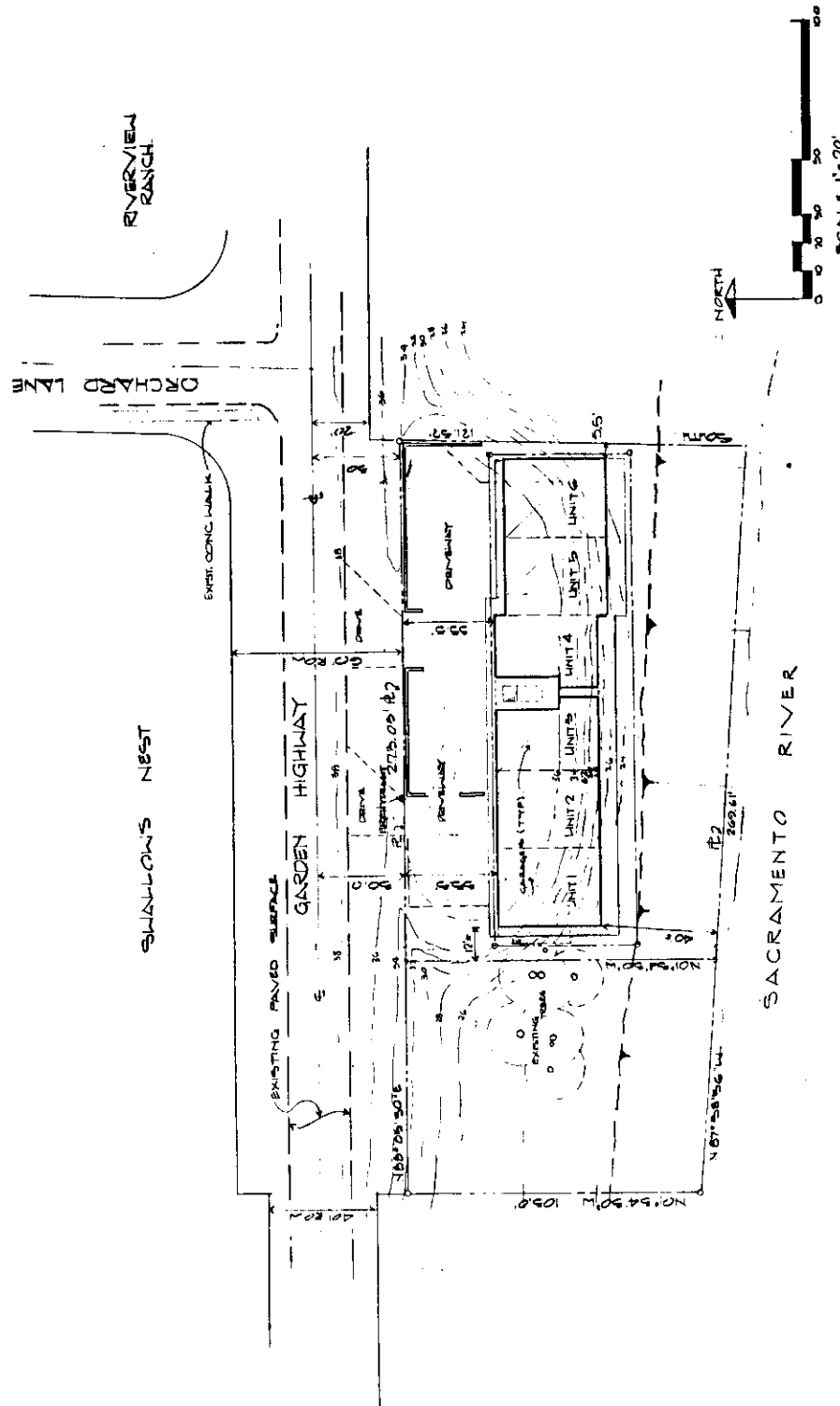
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**WZTWO**

EXHIBIT A

DATE: 5  
 SHEET: 5

**EXHIBIT B**

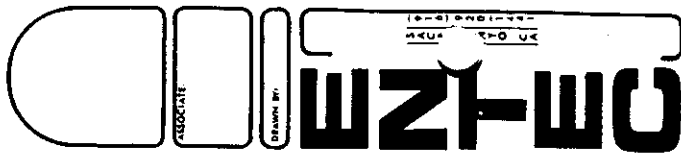


SITE PLAN  
 1"=20'

THE SUTTER KINGS  
CONDOMINIUM  
 LOT 15 OF SHALLOWS NEST, UNIT NO. 1  
 APN 274-270-06

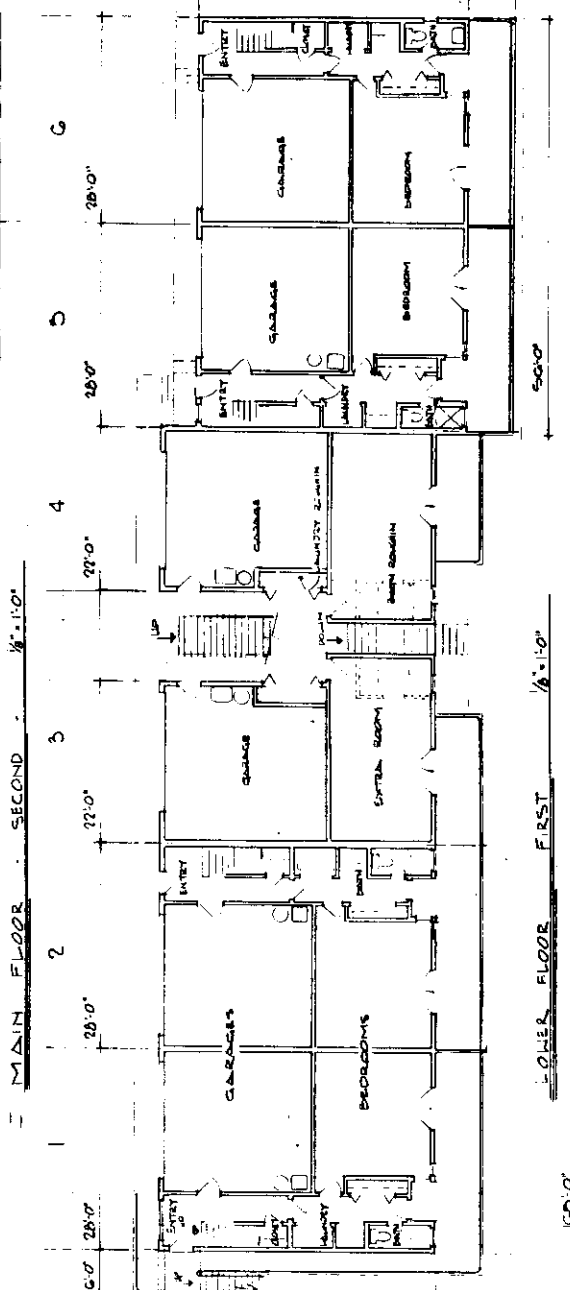
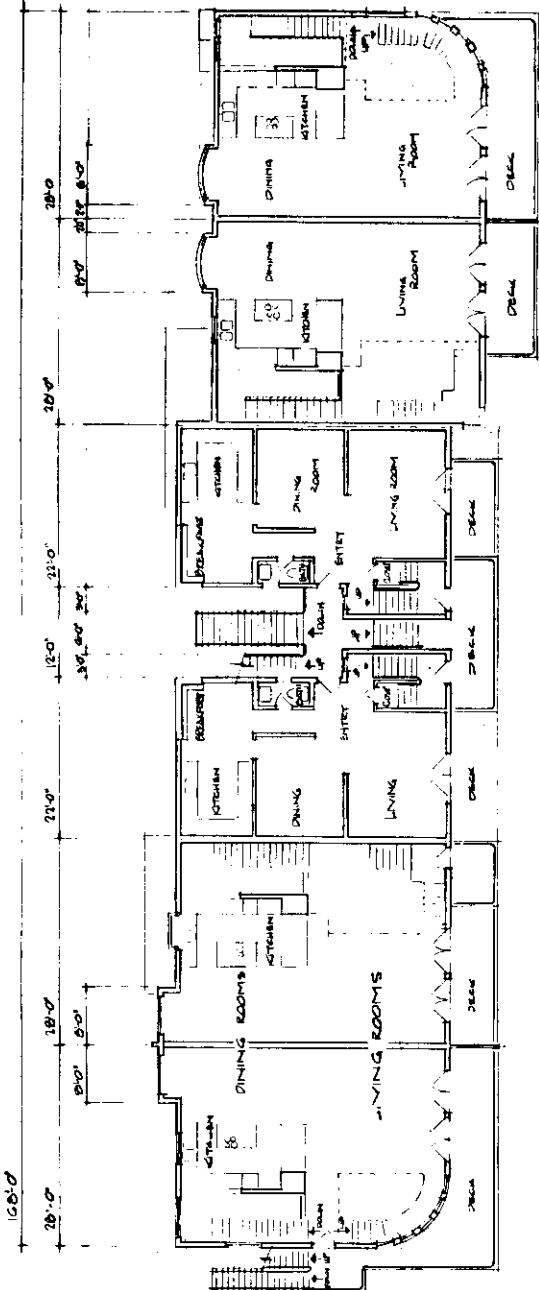
THE SUTTER KINGS  
 SITE PLAN





# EXHIBIT D

DATE	
PART	3
OF	5
THE BUTTER-KINGS	
FLOOR PLANS	



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